



COMHAIRLE NAN EILEAN SIAR

The Town and Country Planning Scotland Act 1997 – Section 36(1)

Town and Country Planning General Development Procedure Order 2013 Regulation 16

Planning Register - Part 1

Application Details

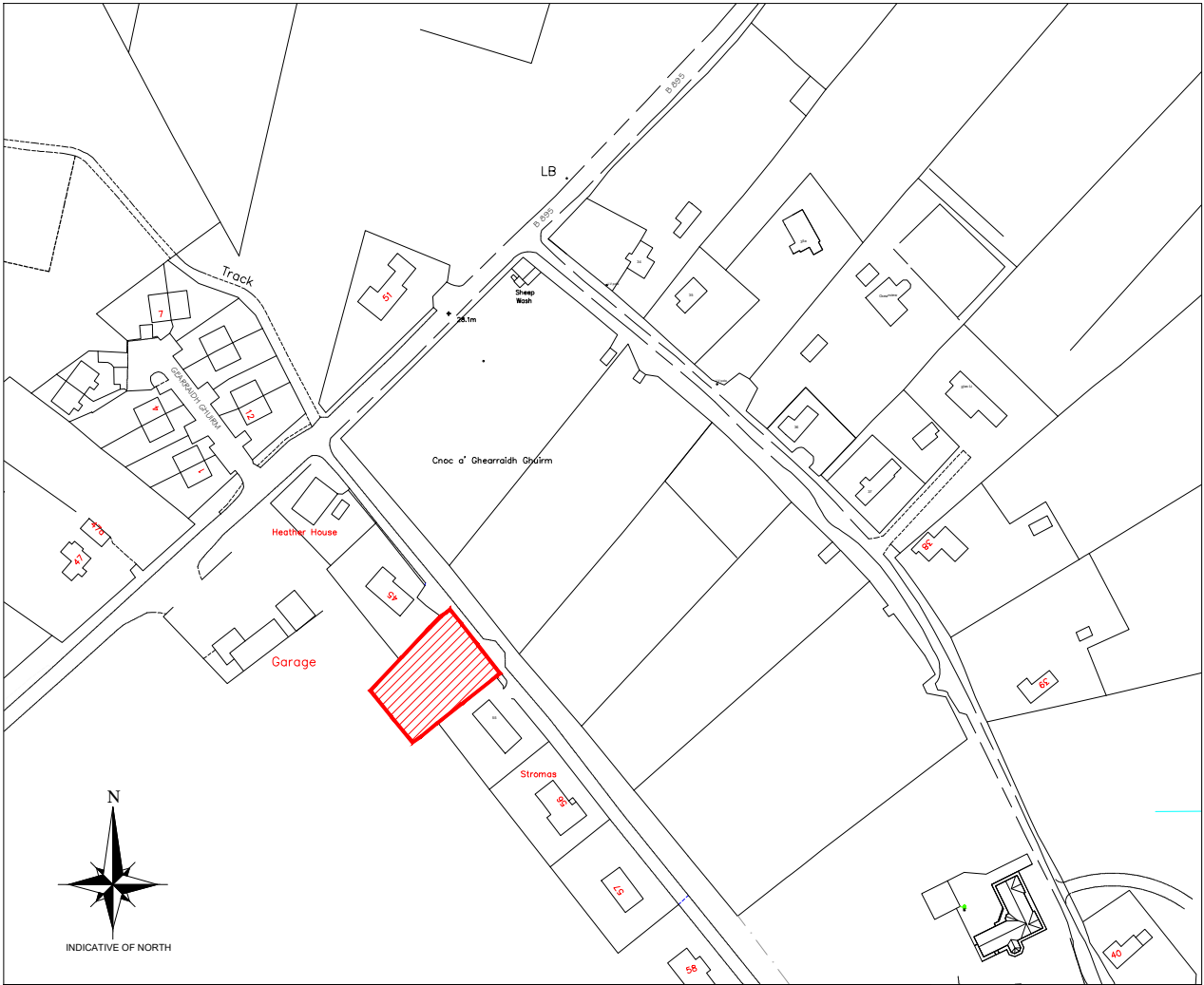
Reference Number	24/00115/PPD
Date registered as valid	18/03/2024
Description of Development	Erect single storey house, with associated access and curtilage
Address or description of location to which the development relates	83 Upper Coll, Back, Isle of Lewis
Co-ordinates	N 937 838 E 145 881
Applicant Name	Mr Iain Martin
Applicant Address	68A Newmarket, Isle of Lewis, HS2 0DU
Agent name (if applicable)	Total Design (Lighthill) Ltd Per Mr David Macleod
Agent Address (if applicable)	Balard, 68 Lighthill, Back, Isle of Lewis, HS2 0LF

The above application summary is accompanied by plans and drawings sufficient to describe the development and where relevant any design statement.

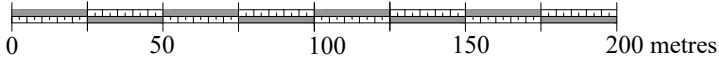
Important Note: on Tuesday 07 November 2023, Comhairle nan Eilean Siar experienced a criminal cyber incident and is working with Police Scotland, the Scottish Government and the National Cyber Security Centre to investigate the matter.

The Online Planning Portal remains unavailable as does our suite of integrated software and hardware systems. In order to enable access by the wider public to application documents and consult upon planning applications, interim systems have been put in place on the temporary website of Comhairle nan Eilean Siar, including a rudimentary facility to display a limited number of documents per application.

Any party wishing to view the application file in full may do so at the offices of Comhairle nan Eilean Siar at Sandwich Road, Stornoway Isle of Lewis, HS1 2BW or Balivanich, Isle of Benbecula. HS7 5LA, ordinarily between 9am and 5pm Monday to Friday (excluding public and local holidays). It is recommended that in advance of visiting an office to view an application that you make an appointment by sending an email to planning@cne-siar.gov.uk



Scale 1:2500



LOCATION PLAN
SCALE 1:2500

Rev	By	Chkd	Apprd	Date	Description
-	-	-	-	-	-

TOTAL DESIGN (LIGHTHILL) LTD

Balard, 68 Lighthill,
Back, Isle of Lewis
HS2 0LF
Tel: 01851 827 137 Mob: 07917 806 013 Email: totaldesign@sky.com

Applicant:
Iain Martin

Project

**45 Upper Coll,
Isle of Lewis**

Drawing

LOCATION PLAN

Drawn by: D MacLeod Date: 13.03.24

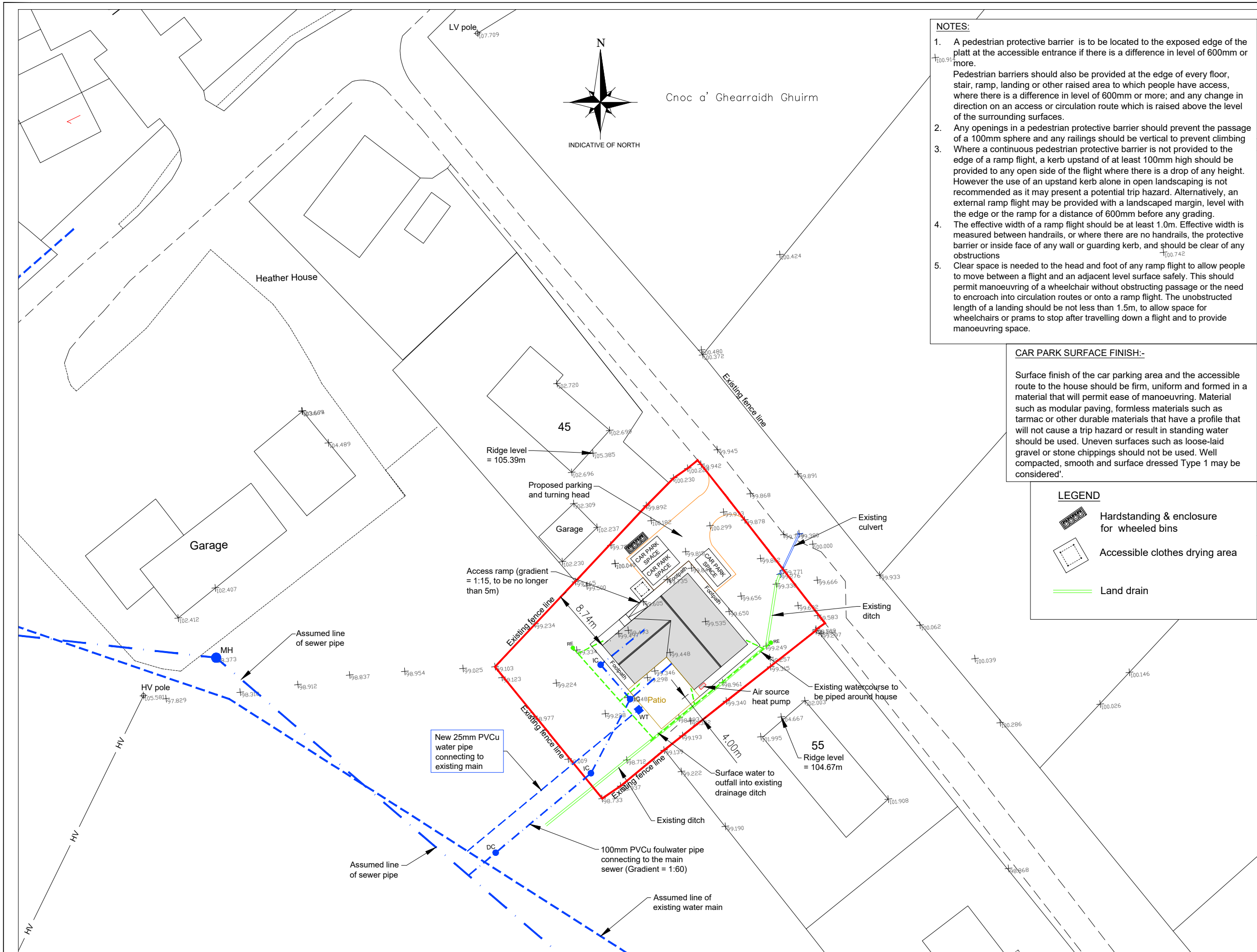
Checked by: - Date: -

Approved by: - Date: -

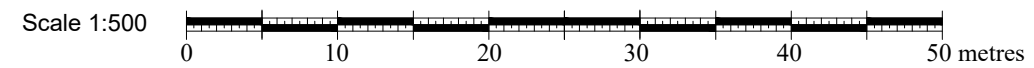
Drawing No. P1505-001 Revision 00

Drawing Scale: AS NOTED @ A3

FOR APPROVAL



SITE PLAN
SCALE 1:500



- NOTES:**
1. A pedestrian protective barrier is to be located to the exposed edge of the platt at the accessible entrance if there is a difference in level of 600mm or more. Pedestrian barriers should also be provided at the edge of every floor, stair, ramp, landing or other raised area to which people have access, where there is a difference in level of 600mm or more; and any change in direction on an access or circulation route which is raised above the level of the surrounding surfaces.
 2. Any openings in a pedestrian protective barrier should prevent the passage of a 100mm sphere and any railings should be vertical to prevent climbing.
 3. Where a continuous pedestrian protective barrier is not provided to the edge of a ramp flight, a kerb upstand of at least 100mm high should be provided to any open side of the flight where there is a drop of any height. However the use of an upstand kerb alone in open landscaping is not recommended as it may present a potential trip hazard. Alternatively, an external ramp flight may be provided with a landscaped margin, level with the edge or the ramp for a distance of 600mm before any grading.
 4. The effective width of a ramp flight should be at least 1.0m. Effective width is measured between handrails, or where there are no handrails, the protective barrier or inside face of any wall or guarding kerb, and should be clear of any obstructions.
 5. Clear space is needed to the head and foot of any ramp flight to allow people to move between a flight and an adjacent level surface safely. This should permit manoeuvring of a wheelchair without obstructing passage or the need to encroach into circulation routes or onto a ramp flight. The unobstructed length of a landing should be not less than 1.5m, to allow space for wheelchairs or prams to stop after travelling down a flight and to provide manoeuvring space.

CAR PARK SURFACE FINISH:-

Surface finish of the car parking area and the accessible route to the house should be firm, uniform and formed in a material that will permit ease of manoeuvring. Material such as modular paving, formless materials such as tarmac or other durable materials that have a profile that will not cause a trip hazard or result in standing water should be used. Uneven surfaces such as loose-laid gravel or stone chippings should not be used. Well compacted, smooth and surface dressed Type 1 may be considered.

- LEGEND**
- Hardstanding & enclosure for wheeled bins
 - Accessible clothes drying area
 - Land drain

- Notes:**
- 1) This drawing to be read in conjunction with the relevant specification and all other relevant drawings.
 - 2) All dimensions and levels to be checked on site and any discrepancies identified prior to commencement of work.
 - 3) All dimensions in metres unless otherwise noted.
 - 4) All levels in metres unless otherwise noted. Levels relative to an approx. OS datum of 48m at survey station 1000.
 - 5) It is the responsibility of the applicant to inform landowners and seek the necessary permission(s) for the proposed foulwater route.
 - 6) It is the responsibility of the applicant/owner to undertake the necessary trial holes to establish if ground conditions are suitable along the extent of the foulwater pipe line.

- LEGEND**
- DC = Disconnecting chamber
 - IC = Inspection chamber
 - MH = Manhole
 - RE = Rodding eye
 - +83.00 = Spot level (m)
 - = rainwater drainage (110mm PVCu pipes) to outfall into soakaway, 1.5m³ per 100m² of roof area
 - = 110mm PVCu foulwater pipe
 - = 110mm PVCu rainwater pipe
 - = Existing water main (2" AC)
 - WT Water toby

FOR APPROVAL

01	DM	10.04.24	HOUSE DESIGN REVISED
Rev	By	Chkd	Apprvd
			Date
			Description

TOTAL DESIGN (LIGHTHILL) LTD
 Baird, 68 Lighthill,
 Back Lane of Lewis,
 HS2 0LF
 Tel: 01851 827 137 Mob: 07917 806 013 Email: totaldesign@sky.com

Applicant:
Iain Martin

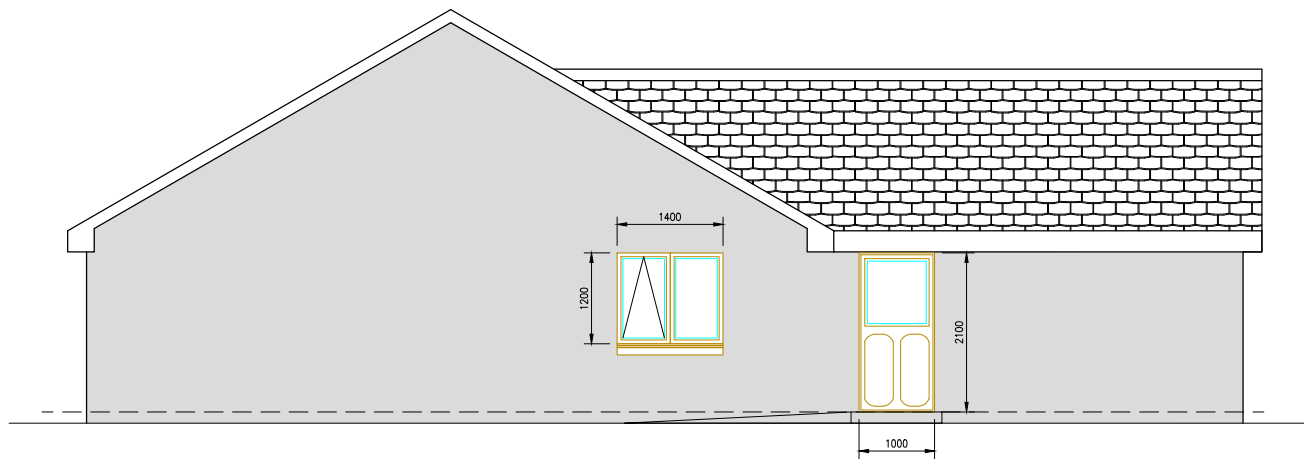
Project
 83 Upper Coll,
 Isle of Lewis

Drawing
SITE PLAN

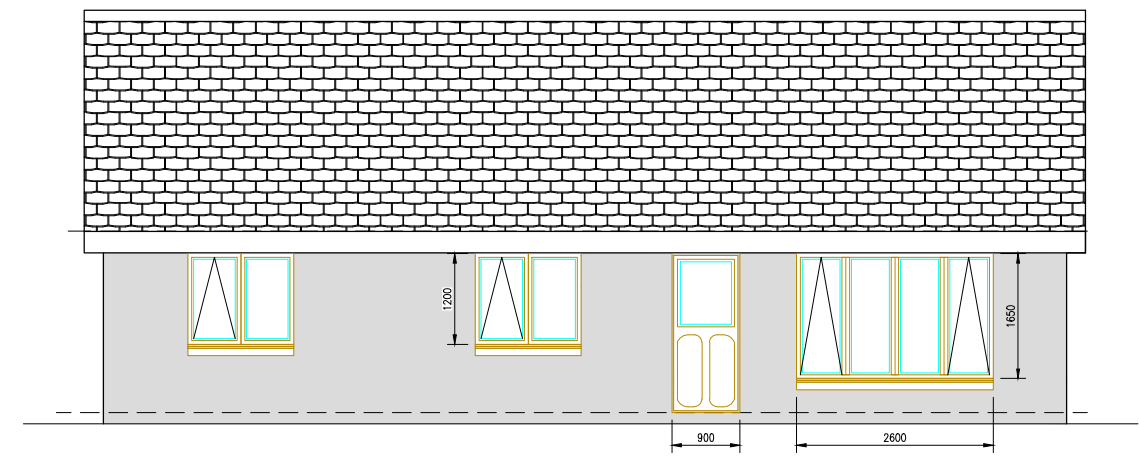
Drawn by: D MacLeod Date: 19.02.24
 Checked by: Date:
 Approved by: - Date: -

Drawing No.	Revision
P1505-002	01

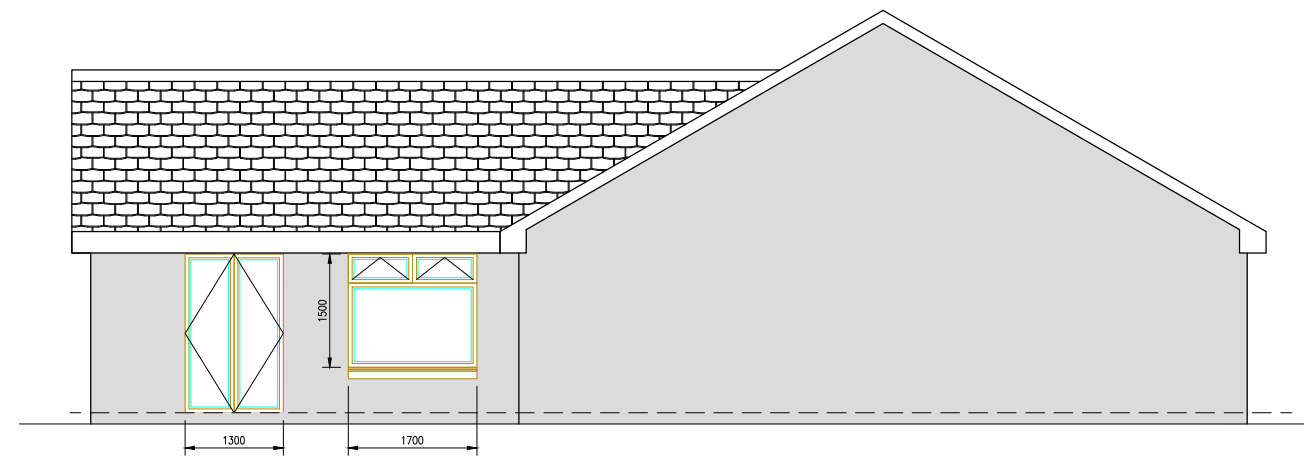
Drawing Scale: AS NOTED @ A3



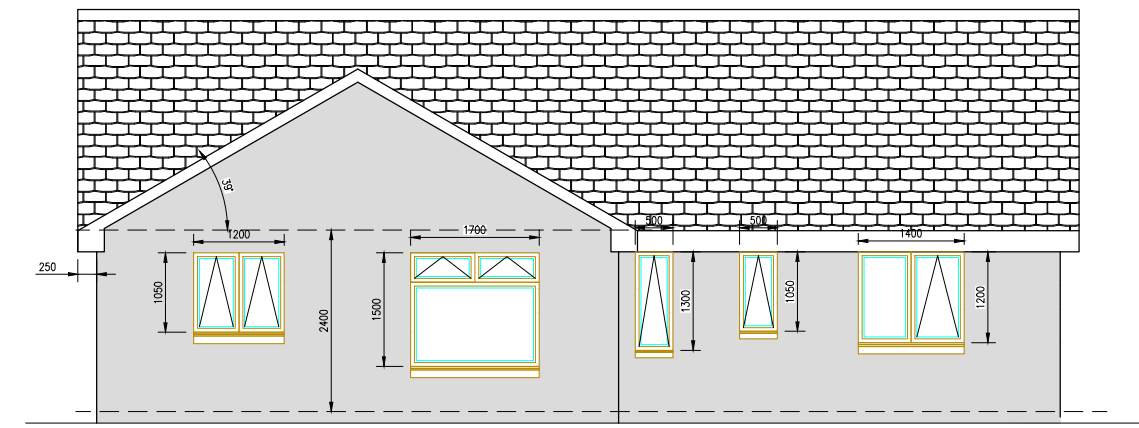
NORTH-WEST ELEVATION
SCALE 1:100



NORTH-EAST ELEVATION
SCALE 1:100



SOUTH-EAST ELEVATION
SCALE 1:100



SOUTH-WEST ELEVATION
SCALE 1:100

EXTERNAL FINISHES:

1. Roof	Concrete roof tiles - Redland Mini Stonewold - black
2. Walls	Dry dash render (Aston cream)
3. Doors/windows	PVCu (anthracite)
4. Fascia & soffit	PVCu (anthracite)
5. Rainwater goods	PVCu (anthracite)

Notes:

- 1) This drawing to be read in conjunction with the relevant specification and all other relevant drawings.
- 2) All dimensions and levels to be checked on site and any discrepancies identified prior to commencement of work.
- 3) All dimensions in metres unless otherwise noted.
- 4) All levels in metres unless otherwise noted. Levels relative to an approx.

01	DM		05.04.24	HOUSE DESIGN CHANGED
Rev	By	Chkd	Apprvd	Date
				Description

TOTAL DESIGN (LIGHTHILL) LTD
 Ballard, 68 Lighthill,
 Back Lane of Lewis
 HS2 0LF
 Tel: 01851 827 137 Mob: 07917 806 013 Email: totaldesign@sky.com

Applicant:
 Iain Martin

Project
 83 Upper Coll,
 Isle of Lewis

Drawing
PROPOSED ELEVATIONS

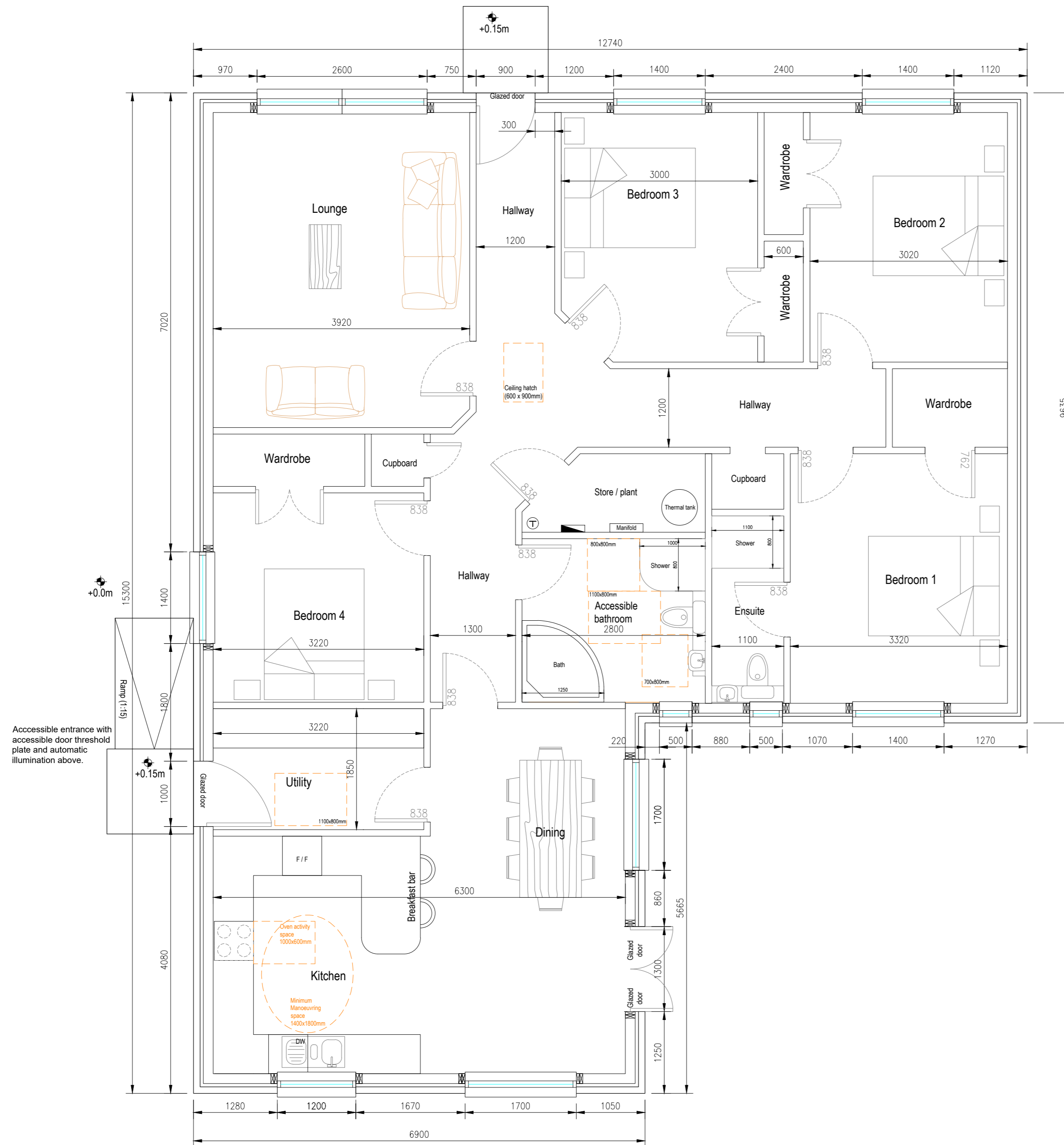
Drawn by: D MacLeod Date: 05.04.24
 Checked by: - Date: -
 Approved by: - Date: -

Drawing No. **S943-003** Revision **01**

Drawing Scale: AS NOTED @ A3

Notes:

- 1) This drawing to be read in conjunction with the Architects drawings, specifications and all other relevant drawings (where any inconsistencies occur clarification should be sought from the Project Manager immediately)
- 2) This drawings to be read in conjunction with the Structural Engineers specification and structural over-marked drawings
- 3) All dimensions and levels to be checked on site and any discrepancies identified prior to commencement of work.
- 4) All dimensions in millimetres unless otherwise noted.
- 5) All levels in metres unless otherwise noted.



FLOOR PLAN
SCALE 1:50

FOR APPROVAL

Rev	By	Chkd	Apprvd	Date	Description
01	DM			04.04.24	NEW HOUSE DESIGN

TOTAL DESIGN (LIGHTHILL) LTD
Balard, 68 Lighthill,
Back, Isle of Lewis
HS2 0LF
Tel: 01851 827 137 Mob: 07917 806 013

Applicant:
Iain Martin

Project
**83 Upper Coll,
Isle of Lewis**

Drawing
**PROPOSED
FLOOR PLAN**

Drawn by: D Macleod Date: 04.04.24

Checked by: - Date: -

Approved by: - Date: -

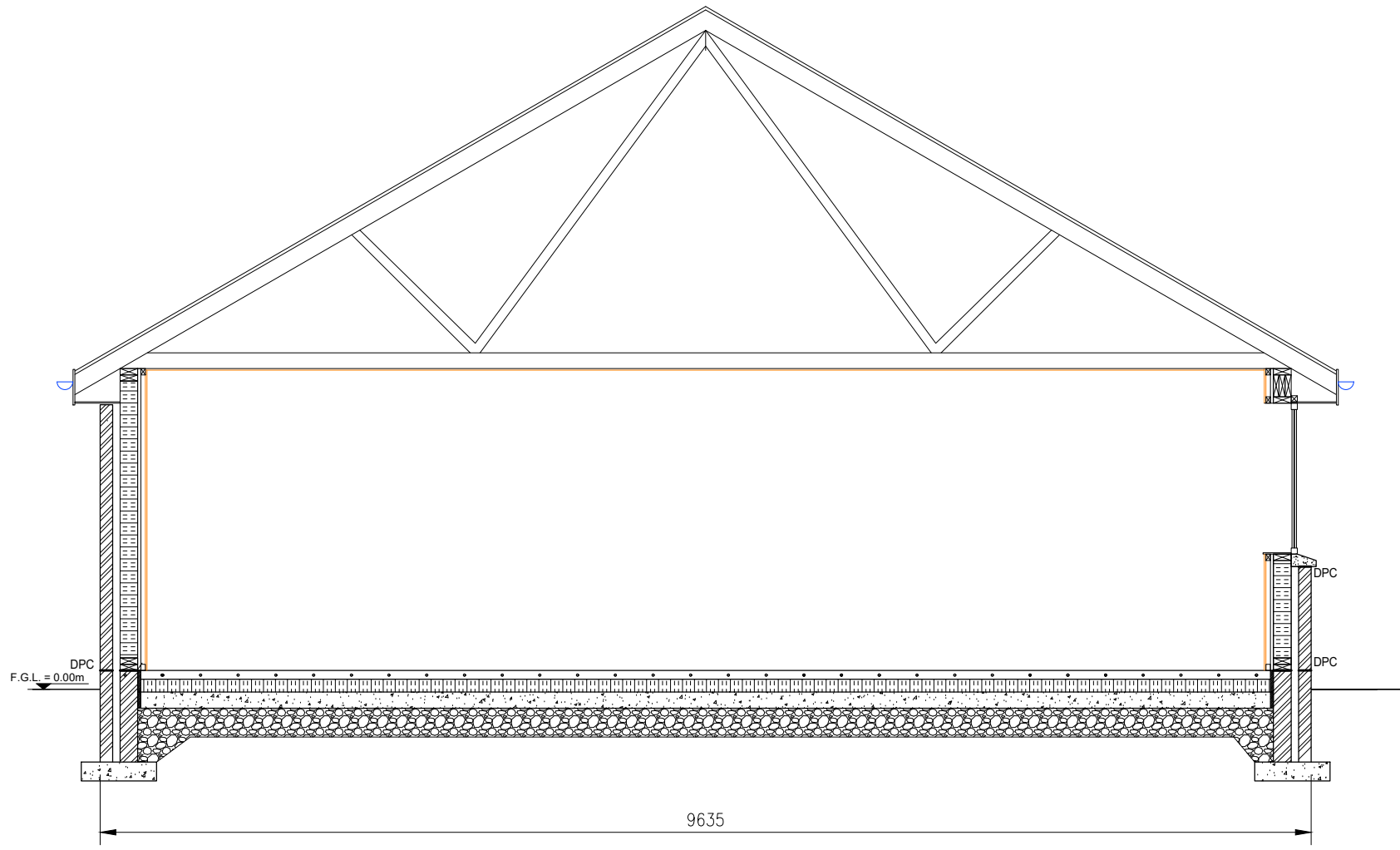
Drawing No. Revision

S943-004 **01**

Drawing Scale: AS NOTED @ A2

Notes:

- 1) This drawing to be read in conjunction with the relevant specification and all other relevant drawings.
- 2) All dimensions and levels to be checked on site and any discrepancies identified prior to commencement of work.
- 3) All dimensions in metres unless otherwise noted.
- 4) All levels in metres unless otherwise noted. Levels relative to an approx.



TYPICAL SECTION
SCALE 1:50

01	DM		05.04.24	HOUSE DESIGN CHANGED	
Rev	By	Chkd	Apprvd	Date	Description

TOTAL DESIGN (LIGHTHILL) LTD
 Ballard, 68 Lighthill,
 Back Lane of Lewis
 HS2 0LF
 Tel: 01851 827 137 Mob: 07917 806 013 Email: totaldesign@sky.com

Applicant:
Iain Martin

Project:
83 Upper Coll,
Isle of Lewis

Drawing:
TYPICAL SECTION

Drawn by: D MacLeod Date: 10.04.24
 Checked by: - Date: -
 Approved by: - Date: -

Drawing No.	Revision
S943-005	01

Drawing Scale: AS NOTED @ A3